

ORDINANCE NO. 2010-16

AN ORDINANCE AMENDING THE TOWN OF CASTLE ROCK ZONING DISTRICT MAP BY CREATING A NEW INTERCHANGE OVERLAY DISTRICT CLASSIFICATION; DESIGNATING THE INTERCHANGE OVERLAY DISTRICT BOUNDARIES; ADOPTING THE INTERCHANGE OVERLAY DEVELOPMENT STANDARDS BY REFERENCE AS A PRIMARY CODE; ADOPTING A NEW CHAPTER 17.16 AND AMENDING 17.12.010 OF THE MUNICIPAL CODE CONCERNING THE INTERCHANGE OVERLAY DISTRICT; AND AMENDING CHAPTER 17.14.070 BY EXEMPTING PROPERTIES WITH AN APPROVED INTERCHANGE OVERLAY PRELIMINARY PLANNED DEVELOPMENT SITE PLAN FROM THE SKYLINE RIDGELINE PROTECTION REGULATIONS

WHEREAS, the Town of Castle Rock is a home rule municipal corporation and the Town Council is empowered to adopt such ordinances as are necessary and convenient to protect the health, safety and welfare of the community,

WHEREAS, the Castle Rock Municipal Code establishes a comprehensive zoning and regulatory framework which prescribes processes, approval criteria, and standards for land use and construction approvals,

WHEREAS, by Resolution No. 2002-138, the Town Council adopted the 2020 Comprehensive Master Plan (Comprehensive Plan) which contains the following provisions:

LU-8.1 Significant natural features, such as buttes, ridgelines and major drainage-ways are critical to the character of Castle Rock and should be respected by all development. The Interchange Districts encourage a high-density build environment within these natural features through alternative form-based zoning regulations. The alternative form-based code will detail how these alternative regulations relate to the existing Municipal Code requirements. Incentives will be provided in the form-based code to promote the high-density, high-quality development desired at these interchanges.

LU-8.2 The desired higher density urban level development will be required to protect significant natural drainage-ways by creating opportunities for natural design and added value; to protect the Plum Creek Corridor and its tributaries; to protect the designated floodplains and designated mouse habitat protection areas; and to integrate water conservation and water quality design concepts into the proposed land use plans.

LU-8.9 Development within Interchange Districts should support Town and regional transportation initiatives by incorporating transit solutions, such as park 'n' ride facilities, and pedestrian/bicycle interconnectivity into proposed development plans. Future light rail or commuter rail stops are desirable within the Interchange Districts. Dedication of land for transit and park 'n' ride uses should occur with approval of plans for land within the Interchange Districts.

LU-8.5 Property owners should be allowed to access increased development rights via new regulatory mechanisms in the form-based code pertaining to required dedications, skyline/ridgeline requirements, intersection spacing on minor arterials, or other relevant regulations, in exchange for such things as contributions to specific gateway enhancements, adherence to increased design standards, cash-in-lieu, and creation of high quality architecture and amenities.

LU-8.3 Public gathering places take the form of plazas and other urban-type gathering areas rather than programmed parks.

LU-5.8 Concentrate commercial and office uses that serve the region and the overall Town where there is a greatest level of accessibility and urban services available.

Principal ED-1 Attract a variety of businesses, industries and employment opportunities to provide a balance in the community between jobs and workers and to diversify the Town's economic base.

LU-6.4 Promote multi-modal access to commercial and retail uses.

LU-8.12 Residences should be integrated into or be located adjacent to Interchange District developments in a manner that promotes pedestrian friendly interconnectivity and promotes alternate means of commuting. In particular, residences to accommodate populations dependent on transit, including affordable housing and housing for seniors, are encouraged.

WHEREAS, the Town Council recognizes the importance of a vibrant and thriving commercial and business sector that provides employment, revenue, goods and services, and which is an integral and essential component of the Castle Rock community,

WHEREAS, it is necessary and appropriate to permit higher density and building heights in the commercial areas surrounding I-25 Interchanges in order to allow for the highest and best use of such properties, and

WHEREAS, public hearings have been held on this Ordinance before the Planning Commission and Town Council as required by Town regulations.

NOW, THEREFORE, IT IS ORDAINED BY THE TOWN COUNCIL OF THE TOWN OF CASTLE ROCK, COLORADO AS FOLLOWS:

Section 1. Zoning District Map Amendment. The official Town of Castle Rock Zoning District Map is amended by the addition of the Interchange Overlay District classification to the properties in the vicinity to the Founders Parkway/Meadows Parkway, Wolfensberger, and Plum Creek Parkway Interchanges. The extent and limits of the Interchange Overlay District are generally depicted in the IO Maps appended to this ordinance. In the event of a discrepancy between the IO map and the

official Zoning District Map, the latter shall control. The underlying zoning district classification of properties within the Interchange Overlay District is unchanged by this map amendment.

Section 2. Amendment. Section 17.12.010 of the Castle Rock Municipal Code is repealed and reenacted to read as follows:

17.12.010 Established.

In order to carry out the provisions of this Title, the Town is divided into the following zoning districts:

- | | | |
|----|-------|---|
| A. | R-1 | Single-Family Residence District (see Chapter 17.20) |
| B. | R-1A | Single-Family Residence District, Detached (see Chapter 17.24) |
| C. | R-2 | Single-Family and Duplex Residence District (see Chapter 17.28) |
| D. | R-3 | Multifamily Residence District (see Chapter 17.32) |
| E. | POS | Parks, Recreation and Open Space District (see Chapter 17.34) |
| F. | MH | Mobile Home District (see Chapter 17.36) |
| G. | B-1 | Highway Oriented Commercial District (see Chapter 17.40) |
| H. | B-2 | General Business District (see Chapter 17.44) |
| I. | I-1 | Light Industrial District (see Chapter 17.52) |
| J. | I-2 | General Industrial District (see Chapter 17.56) |
| K. | PD | Planned Development District (see Chapter 17.60) |
| L. | SR | Skyline/Ridgeline Protection District (see Chapter 17.14) |
| M. | WNZOD | Wolfensberger North Zoning Overlay District (see Chapter 17.50) |
| N. | IO | Interchange Overlay District (See Chapter 17.16) |

Section 3. Adoption of Chapter 17.16. Title 17 of the Castle Rock Municipal Code is amended by the adoption of a new Chapter 17.16 entitled Interchange Overlay District, to read as follows:

**Chapter 17.16
Interchange Overlay District**

- 17.16.010 Adoption of IO Development Standards**
17.16.020 Purpose and effect
17.16.030 Applicability and relationship to underlying zoning
17.16.040 IO process and application of Town regulations
17.16.050 Application of Town regulations
17.60.060 Certain regulations inapplicable
17.60.070 IO Development Standards and variance
17.60.080 Building height
17.60.090 IO PD open space

17.16.010 Adoption of IO Development Standards.

The Town adopts by reference as a primary code the Interchange Overlay Development Standards dated April 21, 2010, published by the Town of Castle Rock Department of Development Services.

17.16.020 Purpose and effect.

The purpose of the Interchange Overlay District (IO) and the Interchange Overlay Development Standards (IODS) is to:

A. Enable the implementation of principles and policies outlined in the Town's 2020 Comprehensive Master Plan concerning development in designated areas in the vicinity of Interstate interchanges.

B. Establish architectural, landscaping, design, building, use and site development regulations that encourage compatible land uses ensure higher quality development with safe and efficient pedestrian and vehicular access.

C. Encourage sustainable quality development through the use of set design and site techniques while continuing to provide a wide range of economic development opportunities.

D. Encourage mixed-use residential, office, commercial, industrial and other compatible uses.

17.16.030 Applicability and relationship to underlying zoning.

This Chapter 17.16 prescribes the process and approval criteria by which properties may obtain the right to develop under the IO zoning classification. Upon obtaining IO approval and execution and recordation of the applicable zoning documents and development agreement, the underlying zoning classification is thereafter extinguished and the zoning of the property is governed exclusively by the new IO zoning and development regulations and development agreement. Unless a landowner seeks and obtains such approval and recordation under this Chapter, development of such property shall be governed by the underlying zoning classification and development agreement, and in such event this Chapter shall have no effect on development of such property.

17.16.040 IO process and application of Town regulations.

Property obtains IO classification through approval of an IO PD. Except as expressly modified by this Chapter 17.16, the process and criteria for application, review and approval of an IO PD is governed by Chapter 17.60 of the Code applicable to PD Planned Development zoning. Accordingly, a landowner must first seek and obtain Town Council approval of a preliminary IO PD site plan and zoning regulations to establish the IO classification for the property.

17.16.050 Application of Town regulations.

All provisions of the Castle Rock Municipal Code apply to property developed under an IO PD, including subdivision and construction, except as expressly modified in this Chapter 17.16, or as otherwise stated in the Code. In the event of a conflict between this Chapter 17.16 and other provisions of the Code, this Chapter 17.16 shall govern and control and to that extent, this Chapter constitutes an amendment to the Code.

The IO Development Standards govern and control over any conflicting provisions of the Town's Public Works Regulations. Except when so preempted, the Public Works Regulations shall be applicable to construction within the IO PD.

IO regulations do not apply to public facilities constructed or developed by the Town of Castle Rock.

17.16.060 Certain regulations inapplicable.

The IO is intended to encourage mixed-use within the same structure or block and/or high intensity commercial development. Accordingly, upon recordation of an approved IO PD:

A. A structure or development within an IO PD that interfaces with another structure or development within the IO PD shall not be required to comply with the provisions of Chapter 17.59 Residential/Non-Residential Interface, however such regulations shall be applied to structures or development within the IO PD which interface with structures or development outside the IO PD.

B. As provided in 17.14.070, Chapter 17.14 Skyline/Ridgeline Protection Regulations does not apply to a property development within an IO PD.

17.16.070 IO Development Standards and variance.

All development within the IO PD shall comply with the IODS whether express reference to such standards is made within the IO PD documents and IO PD zoning regulations.

The Director of Development Services may grant a variance from the IODS upon a finding that:

A. Where, by reason of exceptional shape or topography of a lot, or other exceptional situation or condition of the building or land, practical difficulty or undue hardship, application of the IOZDS creates an undue hardship or impracticality; and

B. The variance request allows the project to achieve the main objectives of the IODS.

17.16.080 Building height.

The maximum building height within an IO PD is 6 stories, or 90-feet, unless a greater height is authorized through the IO PD.

17.16.90 IO PD open space.

The total open space requirement within an IO PD is fifteen percent (15%). The intent is to secure large areas of open space within the IO PD while recognizing the Town's 2020 Comprehensive Plan's development concepts for these areas.

Section 4. Skyline/Ridgeline Protection Regulations Amendment. Section 17.14.070 of the Castle Rock Municipal Code is repealed and reenacted as follows:

17.14.070 Exemptions.

A. Properties that have received final plat approval on or before the effective date of the ordinance codified herein shall be exempt from the provisions of 17.14.060.A and B (including replatting), but shall be subject to all other requirements of this Chapter, including without limitation, the provisions of Section 17.14.060.C.

B. Structures that have been built or for which a building permit has been issued, prior to the effective date of this Chapter, shall be exempt from all requirements of this Chapter, provided that an exempt structure may not be remodeled or expanded so as to result in a structure with a building height in excess of the limitations of Section 17.14.060. An existing structure exempted from the district regulations under this Section may be rebuilt to a building height equal to the greater of: (a) the limitation under 17.14.060; or (b) its prior building height.

C. Structures constructed within an IO PD (see Chapter 17.16) are exempt from this Chapter 17.14.


Section 5. Severability. If any clause, sentence, paragraph, or part of this ordinance or the application thereof to any person or circumstances shall for any reason be adjudged by a court of competent jurisdiction invalid, such judgment shall not affect the remaining provisions of this ordinance.

Section 6. Safety Clause. The Town Council finds and declares that this ordinance is promulgated and adopted for the public health, safety and welfare and this ordinance bears a rational relation to the legislative object sought to be obtained.

APPROVED ON FIRST READING this 1st day of June, 2010 by a vote of -6- for and -0- against, after publication in compliance with Section 2.02.100.C of the Castle Rock Municipal Code; and

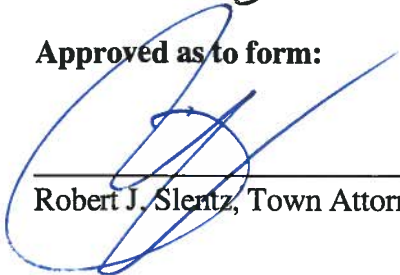
PASSED, APPROVED AND ADOPTED ON SECOND AND FINAL READING this 22nd day of June, 2010, by the Town Council of the Town of Castle Rock by a vote of 6 for and 0 against.

ATTEST:



Sally A. Misare, Town Clerk

Approved as to form:



Robert J. Slentz, Town Attorney

TOWN OF CASTLE ROCK



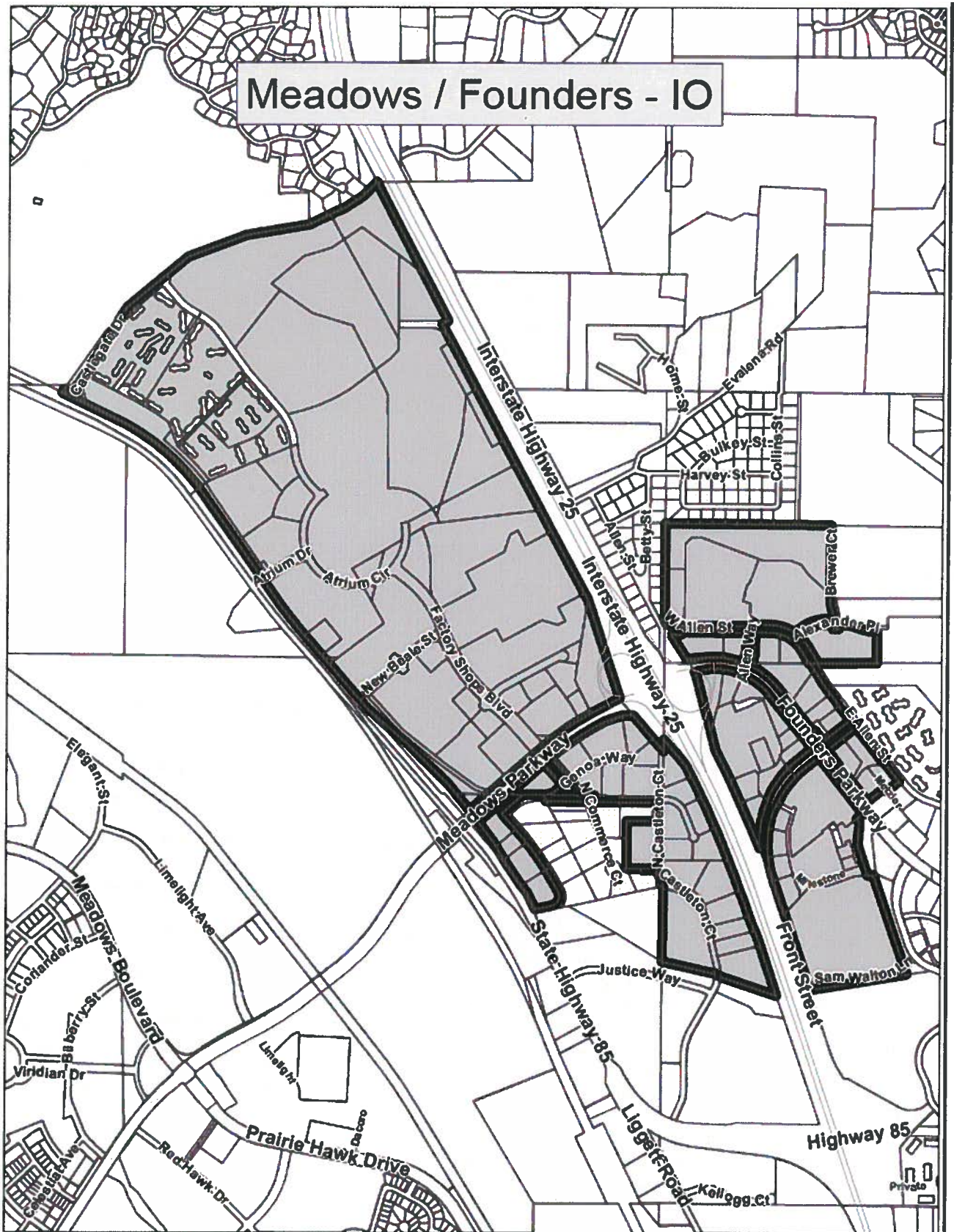
Ryan Reilly, Mayor

Approved as to content:

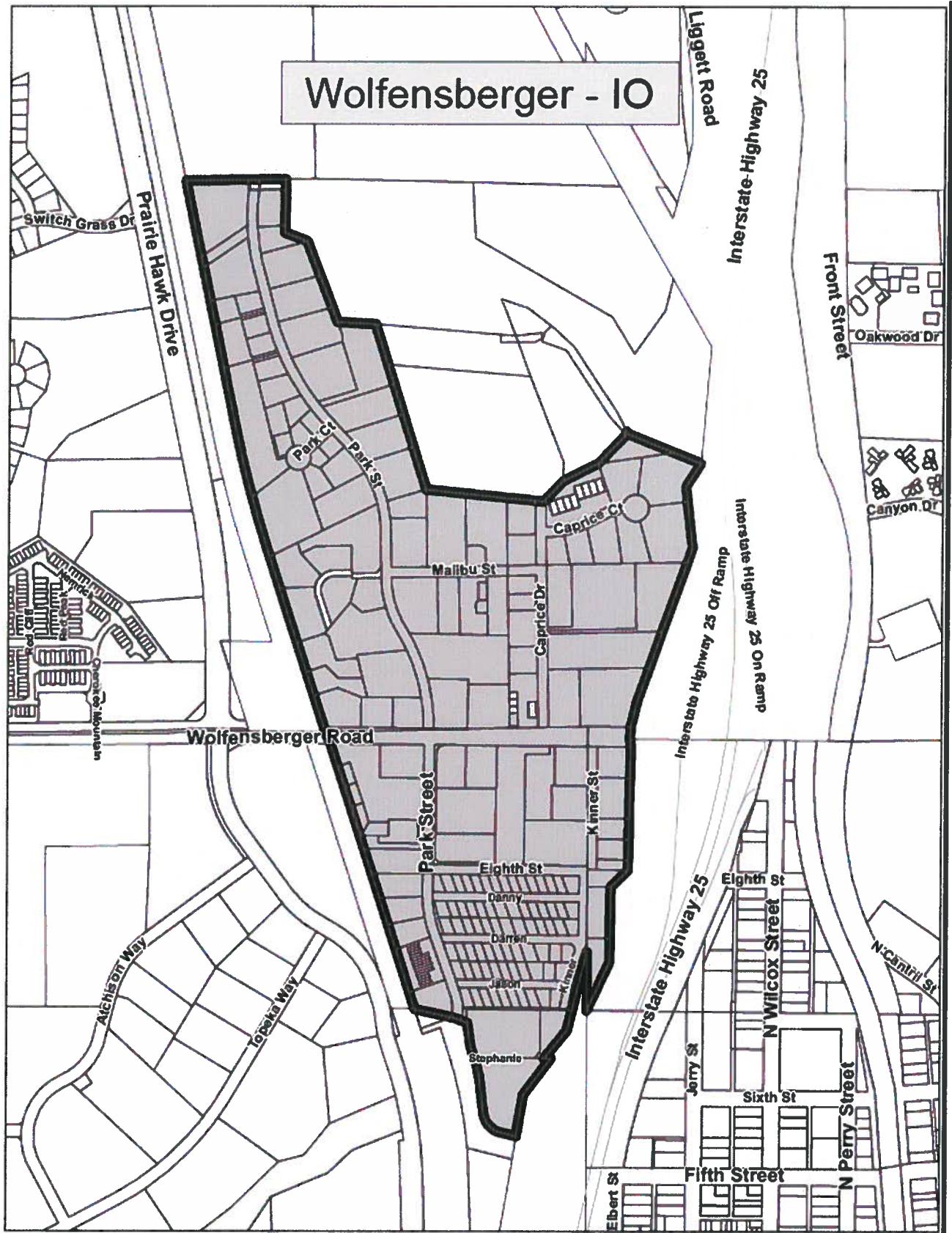


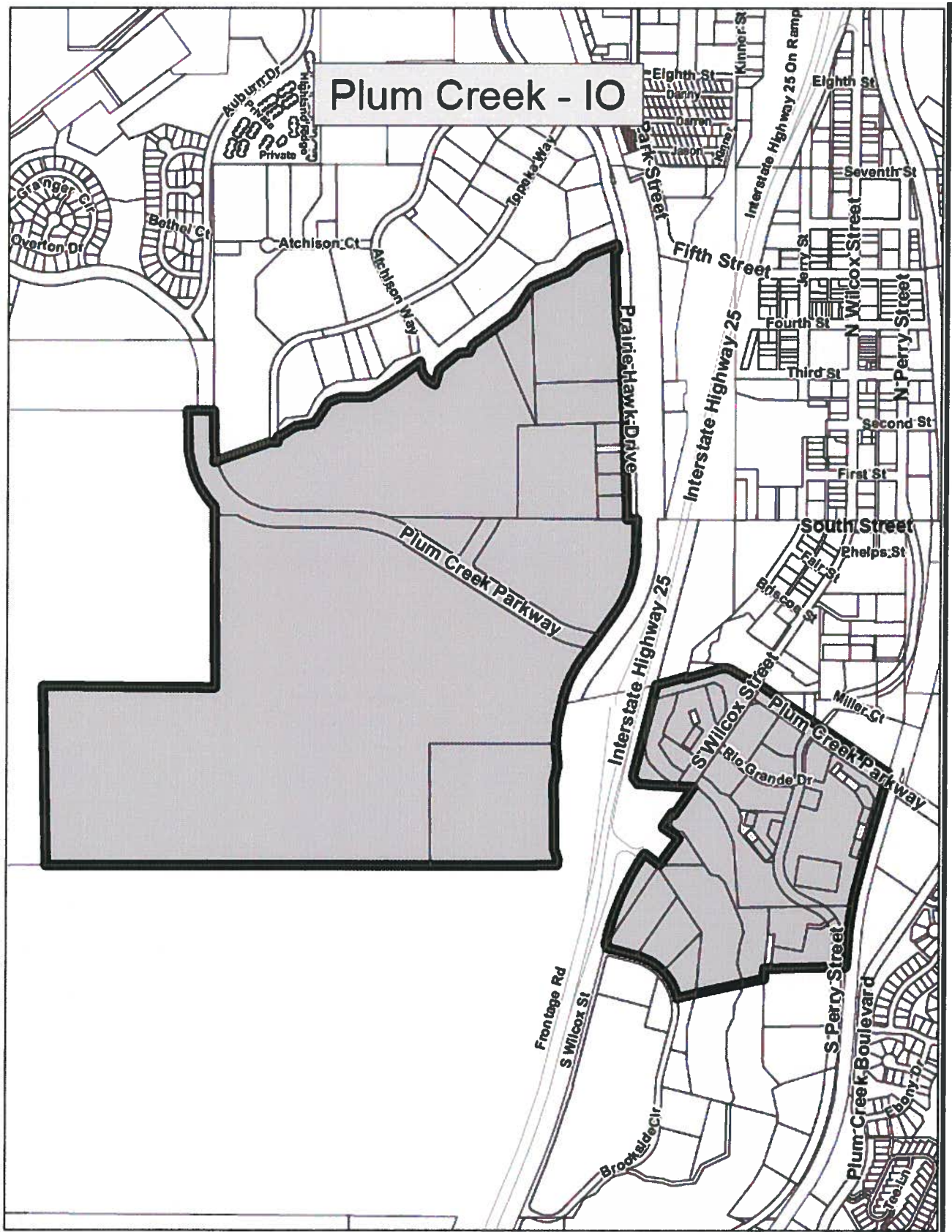
Bill Detweiler, Director of Development Services

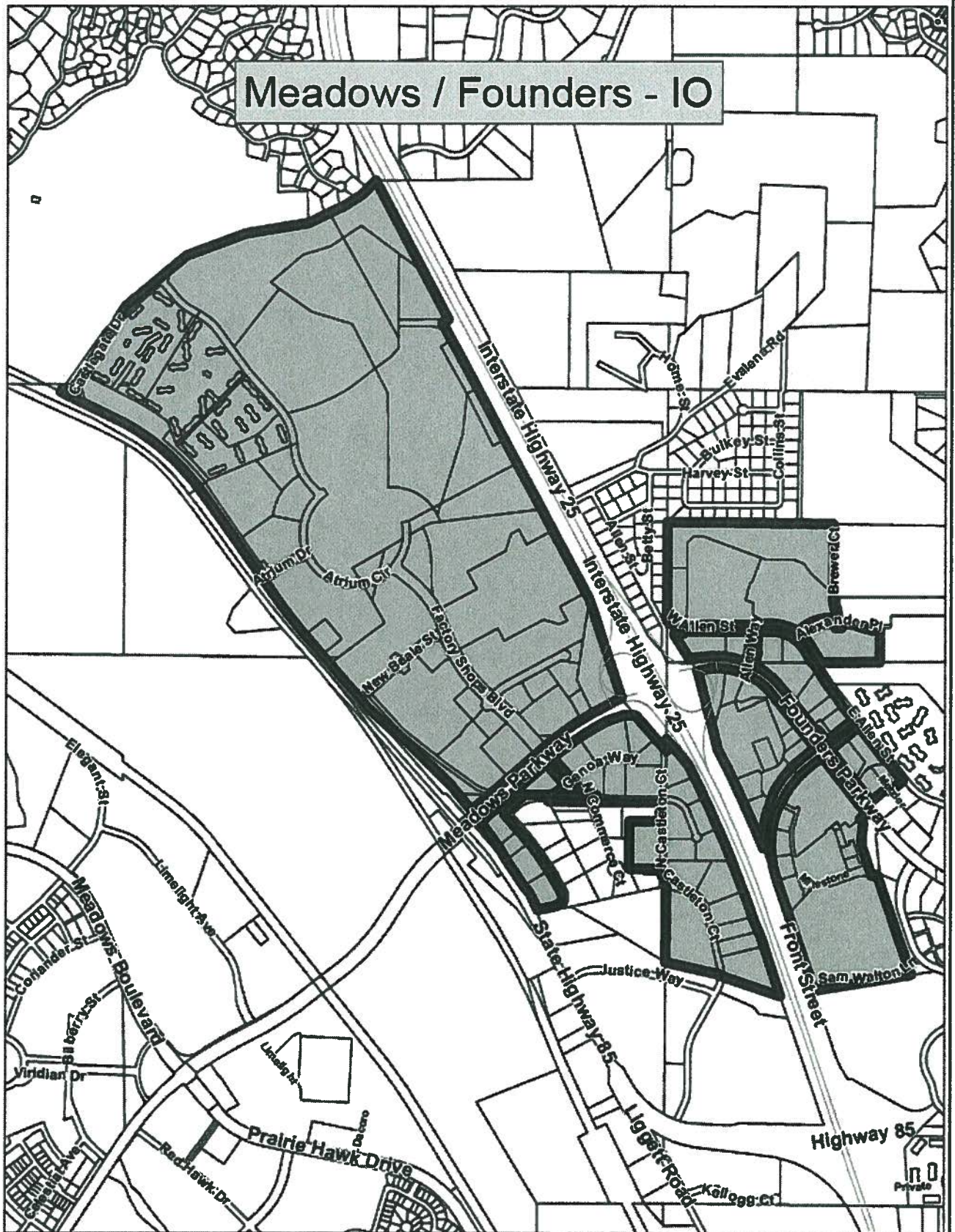
Meadows / Founders - IO

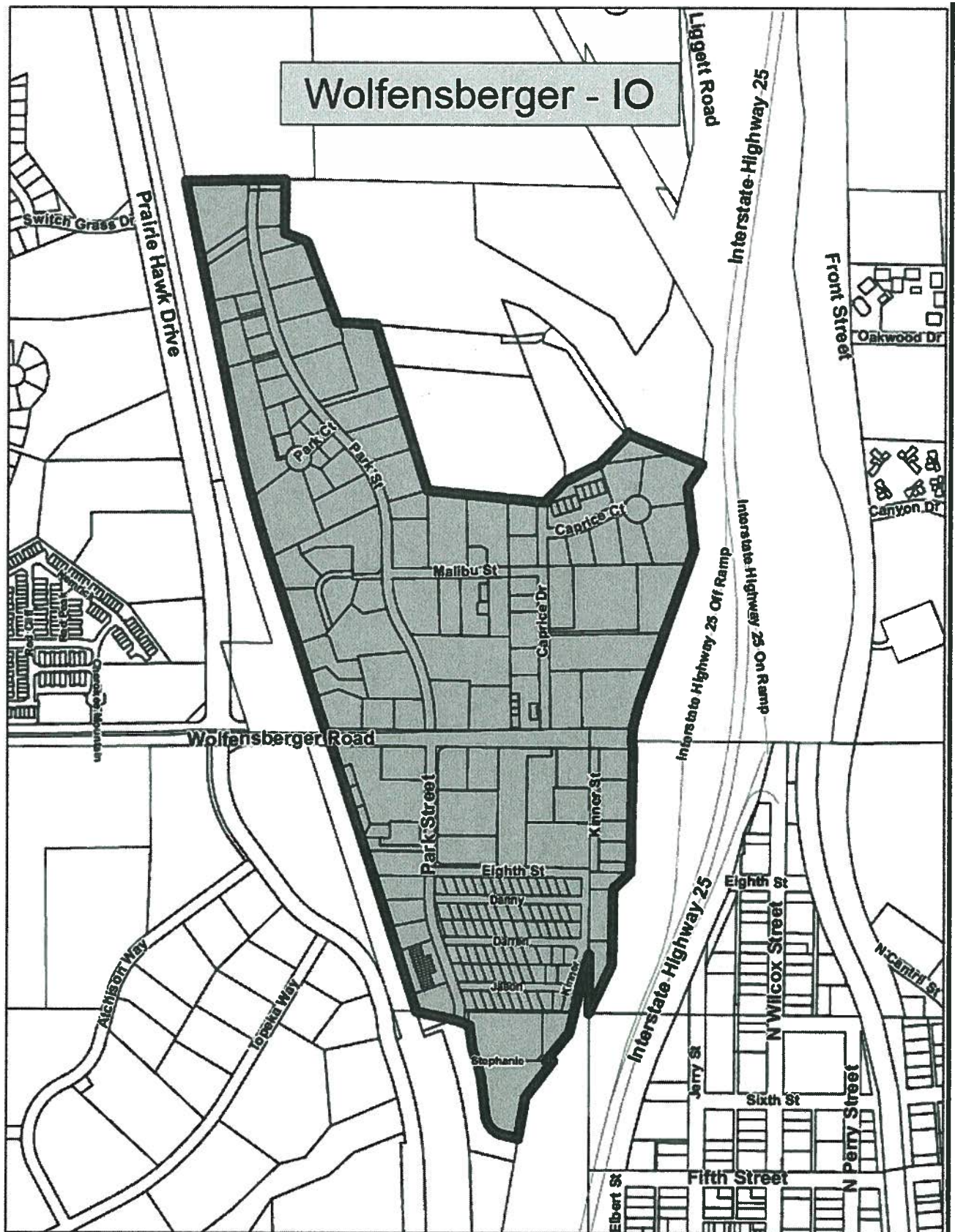


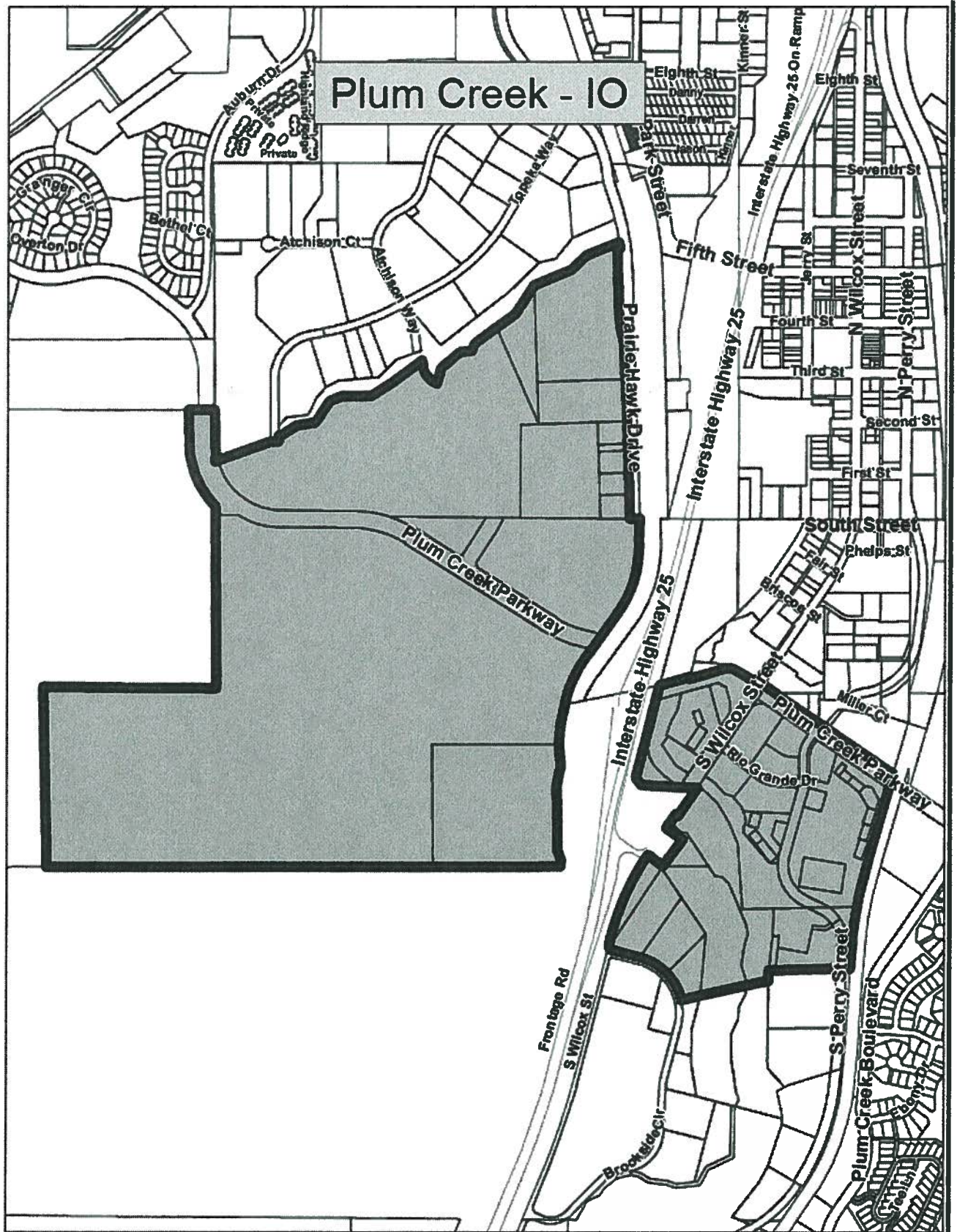
Wolfensberger - IO











Attachment B

Interchange Overlay Development Standards



TOWN OF CASTLE ROCK, CO

Interchange Overlay Development Standards

PUBLISHED ON April 21, 2010 - BY THE TOWN OF CASTLE ROCK'S
DEVELOPMENT SERVICES



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SECTION I. AUTHORITY AND PROCESS
SECTION II. PURPOSE AND INTENT
SECTION III. VARIANCES

Interchange Overlay Development Standards

I. AUTHORITY AND PROCESS

This Code, together with all future amendments, shall be known as the Town of Castle Rock, Colorado, Interchange Overlay Development Standards (IODS). This Code is referenced in Chapter 17.16 of the Town of Castle Rock Municipal Code, pursuant to the powers conferred upon the Town of Castle Rock by the Colorado Revised Statutes. All reference in this document to the "Purpose and Intent" of the Interchange Overlay Development Standards shall rely on Chapter 17.16 of the Municipal Code for direction.

II. PURPOSE AND INTENT

This document, together with the amended municipal code, provides a flexible planning tool that enables development at strategic locations, through the use of enhanced design standards and incentives, to meet the following goals, all formed around the Town's 2020 Vision Statements' Four Cornerstones of, TOWN IDENTITY, COMMUNITY PLANNING, COMMUNITY SERVICES and LOCAL ECONOMY;

1. The creation of a unique identity and image for 'gateway' areas into the Town.
2. Strengthening of the Town's tax base by the expansion of economic activity through diversity and balance of housing, services, and employment.
3. Generation of additional employment opportunities.
4. Encourage the development/re-development of economically underutilized land.
5. Protect natural features.
6. Create pedestrian/non-motorized connectivity, through the implementation of an interconnected framework of transit, pedestrian, bicycle systems, trails and green corridors .
7. Establish the Town as a stand-alone, self-supporting, economic business center.
8. Ensure high-quality new development in the form of high intensity and high quality, both in land use and architecture to provide a mixture of employment, retail, and civic uses. A residential component shall be encouraged.
9. Incorporate buildings and streetscaping that contribute to the physical definition of streets, alleys, sidewalks and bikeways.
10. The use of energy conservation features and 'green' building techniques.

III. VARIANCES

1. See Chapter 17.16 of the Town's Municipal Code.

SECTION IV. INTERCHANGE OVERLAY DISTRICT

Interchange Overlay Development Standards

IV. INTERCHANGE OVERLAY DISTRICT

A. APPLICATION AND DESCRIPTION

1. The following areas, which are proposed and preferred for high quality, high intensity commercial development, are allowed to utilize the IODS upon the adoption by the Town Council of a zoning map amendment.

- Meadows/Founders
- Wolfensberger
- Plum Creek
- And Other Areas Approved By Town Council


2.

10

General Character: Shops mixed with townhouses, larger Apartment houses, Commercial Activity, Offices, workplace, and civic buildings; predominantly attached buildings; trees within the public right-of-way; substantial pedestrian activity.

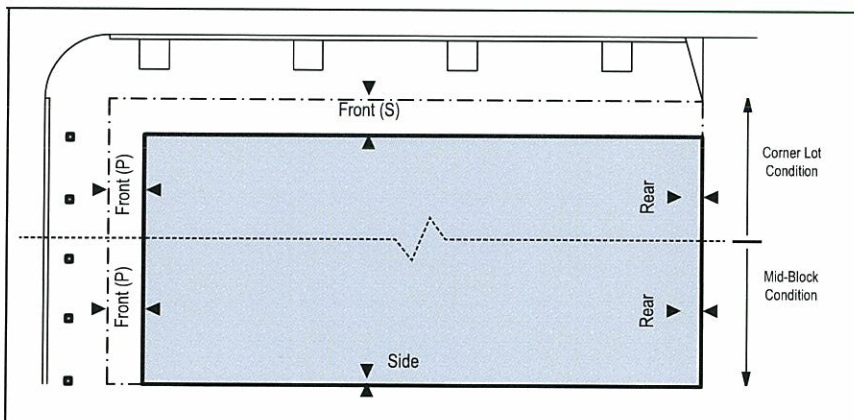
Building Placement: Shallow setbacks or none; buildings oriented to street defining a street wall.

Intensity: Controlled by building height and parking standards.



B. BUILDING SITUATION

1. SETBACKS



PREFERRED SETBACKS - PRINCIPAL BUILDING	
Front Setback Principal (P)	0 ft. min.
Front Setback Secondary (S)	0 ft. min.
Side Setback	0 ft. min.
Rear Setback	3 ft. min.

SECTION IV. INTERCHANGE OVERLAY DISTRICT

Interchange Overlay Development Standards

2. PRIVATE FRONTAGE

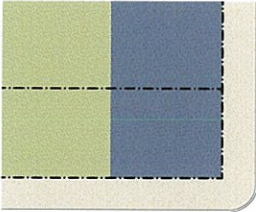
	SECTION		PLAN	
	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE
<p>Forecourt: a Frontage where a portion of the facade is close to the Frontage Line and the central portion is set back. This type should be distributed sporadically amongst other Frontage types. Large trees within the forecourts may overhang the sidewalks to provide additional shade.</p>				
<p>Stoop: a Frontage where the facade is aligned close to the Frontage Line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing.</p>				
<p>Shopfront: a Frontage where the facade is aligned close to the Frontage Line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning can overlap the sidewalk with a right of way encroachment permit.</p>				
<p>Gallery: a Frontage where the facade is aligned close to the Frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk with a right of way encroachment permit. This type is conventional for retail use. The gallery shall be no less than 10 feet wide and should overlap the sidewalk to within 2 feet of the curb.</p>				
<p>Arcade: a Frontage where the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the Frontage Line. This type is conventional for retail use. The arcade shall be no less than 12 feet wide and should overlap the sidewalk to within 2 feet of the curb.</p>				

SECTION IV. INTERCHANGE OVERLAY DISTRICT

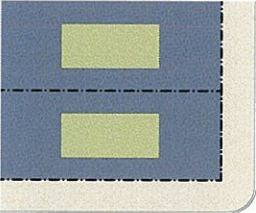
Interchange Overlay Development Standards

3. **PERMITTED TYPES**

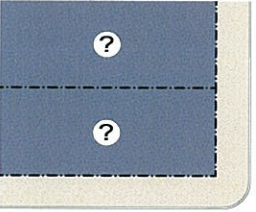
Rearyard: A building that occupies the full Frontage. The rearyard can accomodate substantial parking.



Courtyard: A building that occupies the boundaries of its lot, while internally defining one or more private patios.



Flexible: Specifically designed to meet the intent of these standards. Buildings shall be unique in design and appearance and be in compliance with these standards.



4. **PERMITTED HEIGHT**

MAXIMUM BUILDING HEIGHTS
Height restrictions are set by Chapter 17.16 of the Castle Rock municipal code and/or Town Council approved IO PD. Height will be messured from the dirt grade to the top of the roof.

SECTION IV. INTERCHANGE OVERLAY DISTRICT

Interchange Overlay Development Standards

C. PROPOSED PERMITTED USES

1. These permitted uses should be used as a guide for establishing IO PD allowed permitted uses. The goal is to ensure mixed use development in an IO PD.

By Right <input type="checkbox"/>	CIVIC BUILDINGS	LIGHT INDUSTRIAL
RESIDENTIAL	Bus Shelter <input type="checkbox"/>	Manufacturing <input type="checkbox"/>
Mixed Use Block <input type="checkbox"/>	Convention Center <input type="checkbox"/>	Laboratory Facility <input type="checkbox"/>
Apartment Building <input type="checkbox"/>	Conference Center <input type="checkbox"/>	Fabrication <input type="checkbox"/>
Live/Work Unit <input type="checkbox"/>	Recreation Center <input type="checkbox"/>	Wholesaling <input type="checkbox"/>
Row House <input type="checkbox"/>	Fountain or Public Art <input type="checkbox"/>	Research and Development <input type="checkbox"/>
LODGING	Library <input type="checkbox"/>	OTHER
Hotel <input type="checkbox"/>	Live Theater <input type="checkbox"/>	Gasoline Station <input type="checkbox"/>
Inn <input type="checkbox"/>	Movie Theater <input type="checkbox"/>	Drive-Through Facility <input type="checkbox"/>
Bed & Breakfast <input type="checkbox"/>	Museum <input type="checkbox"/>	Shopping Center <input type="checkbox"/>
OFFICE	Outdoor Auditorium <input type="checkbox"/>	Shopping Mall <input type="checkbox"/>
Office Building <input type="checkbox"/>	Parking Structure <input type="checkbox"/>	Childcare Center <input type="checkbox"/>
Live/Work Unit <input type="checkbox"/>	Passenger Terminal <input type="checkbox"/>	
Flex Building <input type="checkbox"/>	College and Dormitory <input type="checkbox"/>	
RETAIL	Sports Stadium <input type="checkbox"/>	
Open-Market Building <input type="checkbox"/>	Surface Parking Lot <input type="checkbox"/>	
Retail Building <input type="checkbox"/>	Religious Assembly <input type="checkbox"/>	
Art Gallery <input type="checkbox"/>	CIVIL SUPPORT	
Restaurant/Bar <input type="checkbox"/>	Funeral Home <input type="checkbox"/>	
Liquor Store <input type="checkbox"/>	Hospital <input type="checkbox"/>	
First Floor Manufacturing <input type="checkbox"/>	Medical Clinic <input type="checkbox"/>	

2. **Use By Special Review**
- a. Uses outside of the categories outlined above and/or are within the IO PD permitted uses, which include, but are not limited to the following may be granted subject to an application through for a Use by Special Review.

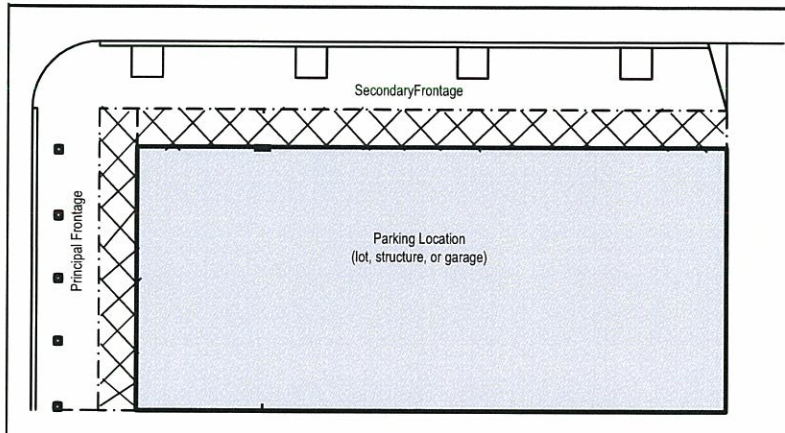
Use By Special Review <input type="checkbox"/>
OTHER
Cemetery <input type="checkbox"/>
Electric Substation <input type="checkbox"/>
Wireless Cell Tower <input type="checkbox"/>
Car Sales <input type="checkbox"/>


SECTION V. PARKING STANDARDS

Interchange Overlay Development Standards

V. PARKING STANDARDS

A. PARKING PLACEMENT



 Parking setback area. Minimum 10 feet.

1. Parking shall be accessed by alleys or rear lanes, when such are available.
2. Open parking areas shall be masked from the Frontage by a building or streetscreen.
3. Bicycle racks shall be provided for of all buildings.

B. REQUIRED PARKING

USE	PARKING
Residential	2 / dwelling
Office (non-medical)	4 / 1000 sq. ft.
Retail	3.5 / 1000 sq. ft
Hotel/Lodging	1 / Bedroom
Restaurant	1 / 3 seats
Medical Office	4.5 / 1000 sq. ft.

NOTE: Other uses not listed above will determine their parking requirements through the IO PD process.

1. The standard parking requirements for a Mixed-use development may be reduced by a calculation of shared parking requirements for the development, utilizing an accumulation factor table based upon the proportions contained in the following table to access peak period parking requirement.
2. The total aggregate reduction in the minimum-parking requirement for uses shall not exceed 20 percent of the standard conventional parking requirement or as approved by the Development Services Director. Car parking stalls located in front of residential garages or within individual residential unit garages or allocated or designated parking spaces shall not be included in any shared parking calculation.
3. On-street parking that is directly adjacent to the lot frontage may be counted as part of the parking provision.

SECTION V. PARKING STANDARDS
SECTION VI. ROADWAY HIERARCHY AND STREET STANDARDS

Interchange Overlay Development Standards

C. SHARED PARKING CALCULATION (TABLE A)

USE	WEEKDAY	EVENING	NIGHT	WEEKEND
Residential	45%	75%	100%	100%
Office (non-medical)	100%	40%	5%	10%
Retail	95%	100%	5%	100%
Hotel/Lodging	95%	75%	100%	100%
Restaurant	100%	100%	15%	100%
Medical Office	100%	90%	10%	80%

NOTE: Other uses not listed above will determine their parking requirements through the IO PD process.

- Table A is used to calculate the demand based on the appropriate % of the conventional parking requirement at specified periods of the day. The highest resultant Mixed-use accumulation is used to determine the maximum shared parking demand. An example on how to apply the shared parking calculation can be found at Section X of this document.

VI. ROADWAY HIERARCHY AND STREET STANDARDS

A. STANDARDS

- Unless specifically modified herein, the design and construction of all streets, alleys, sidewalks and bikeways shall conform to the Town's Public Works Regulations and the Town's Transportation Master Plan.
- Cul-de-sacs are not recommended.

B. PUBLIC FRONTAGE

- The Public Frontage contributes to the character of each Interchange Overlay District, and includes the sidewalk, curb, planter, bike lanes and street trees.
- The spacing for planting in the Public Frontages may be adjusted to accommodate specific site conditions, such as the spacing of trees to avoid visually obscuring the shopfronts.
- The Public Frontage shall include trees and understory planting materials of various species, shall conform to the Town of Castle Rock's water conservation standards, Landscape Regulations, and the streetscape standards.
- The introduced landscape shall consist primarily of native species requiring minimal irrigation, fertilization and maintenance.

SECTION VI. ROADWAY HIERARCHY AND STREET STANDARDS

Interchange Overlay Development Standards

C. ROADWAY HIERARCHY (TABLE B)

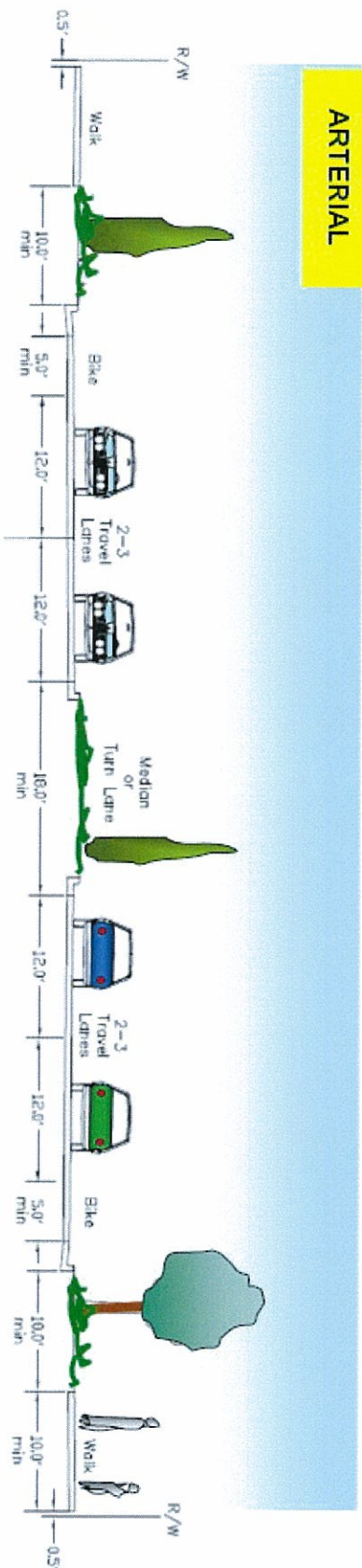
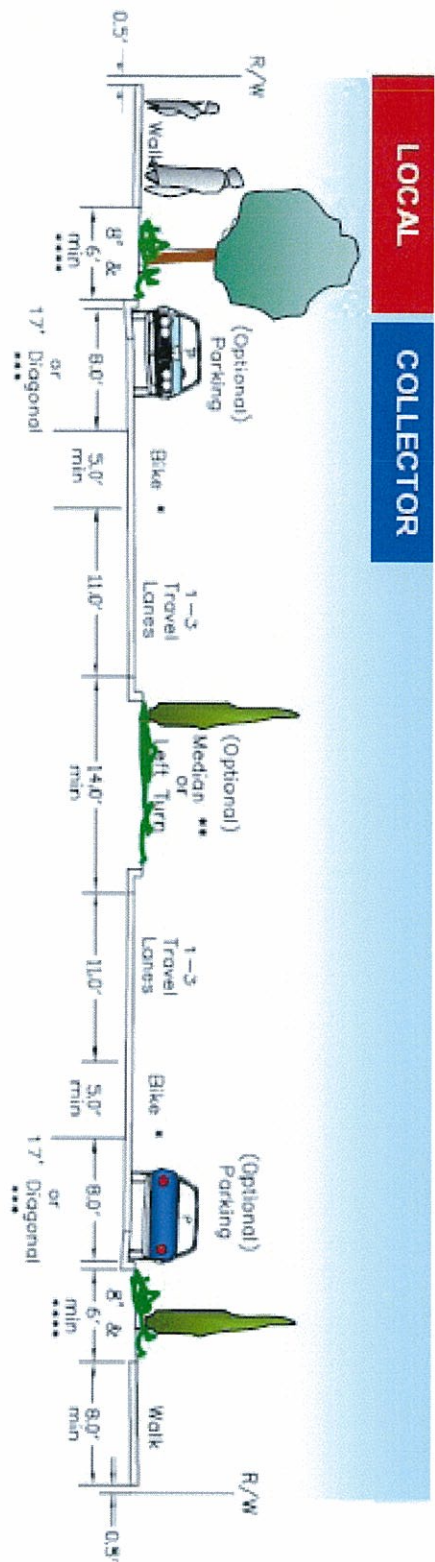
REQUIRED STANDARDS	ARTERIAL	COLLECTOR	LOCAL	ALLEY
ROW minimum	122	84	56	25
FL to FL minimum	80	50	26	20 (note)
Design Speed	40	30	20	5
Number of Travel Lanes (min-max)	4 - 6	2 - 4	2	2
Travel Lane Minimum	12	11	11	10
Travel Lane (with angled parking)	X	X	11	X
Median minimum	18	14	OPTIONAL**	X
Turn Lane minimum	12	12	OPTIONAL	X
On-Street Parking (parallel)	X	8	8	X
On-Street Parking (angled)	X	X	17	X
Bike Lane minimum	5	5	5*	X
Streetscape area minimum	10	8	6	X
Sidewalk minimum	10	8	8	X
Curb Extensions with parking	X	YES	YES	X
Corner Radii minimum	25 (R)	20 (R)	20 (R)	X
Exclusive Left Turn Lane Allowed	YES (R)	YES (R)	YES (R)	X
Pedestrian Islands	YES (R)	YES (R)	YES (R)	X

D. ROADWAY HIERARCHY STANDARDS

- Unless specifically modified herein, the design and construction of all streets, alleys, sidewalks and bikeways shall conform to the Town's Public Works Regulations and the Town's Transportation Master Plan.
- Table B is used in conjunction with the attached roadway design cross sections, the roadway design cross sections may be specified based on the parameters of the roadway hierarchy Table B .
- Each type of roadway hierarchy (**ARTERIAL, COLLECTOR, LOCAL**) may only connect to a roadway type within that specification or to a roadway type in a directly adjoining specification (excepting **ALLEY**).
ARTERIAL can connect with **COLLECTOR**.
COLLECTOR can connect with **ARTERIAL** and **LOCAL**.
LOCAL can connect to **COLLECTOR**.
ALLEY can connect to **COLLECTOR** and **LOCAL**.
- (R)** indicates that roundabout integration is encouraged as an alternative design solution. The final design of any roundabout will be subject to traffic volume and intensity of land use and sign-off by the Town's Public Works division.
- ALLEY** may have inverted crowns and not flowlines.
- Refer to Cross Sections for the following:
* Bike lanes are required on **COLLECTOR** and **LOCAL** roads that intersect with **COLLECTOR** roads.
* Bike lanes not permitted between diagonal parking and drive lanes.
** Median required on **LOCAL** roads that intersect with **COLLECTOR** roads.
- Narrow street sections with on street parking will have to comply with Fire Department emergency access requirements.
- The appropriate sight-line/sight-triangle will need to be accommodated at intersections where buildings are in proximity to the back edge of the sidewalk.

SECTION VI. ROADWAY HIERARCHY AND STREETS STANDARDS

E. INTERCHANGE OVERLAY DISTRICT ROADWAY CROSS SECTIONS



SECTION VI. ROADWAY HIERARCHY AND STREET STANDARDS

Interchange Overlay Development Standards

F. UNDERGROUND UTILITIES

1. Underground utilities shall conform with the Town of Castle Rock's Public Works Regulations unless otherwise provided.

ALLEY

2. Alleys may be utilized in tandem with a parallel local street for the provision of underground wet utilities. Typically, only one utility shall be permitted within an alley. This shall be limited to either storm or sanitary sewer. If two wet utilities are required within an alley, there shall be a maximum diameter of 18" for each pipe.

SECTION VII. STREETScape AND LANDSCAPE

Interchange Overlay Development Standards

A. STREETScape STANDARDS

1. GENERAL

- a. Unless specifically modified herein, the design of streetscapes shall conform to the Town's Landscape Regulations and/or approved IO PD.
- b. Continuous planters shall be provided along all streets without on street parking.
- c. Intermittent planters shall be utilized along commercial streets with parallel on street parking.
- d. The widths of the planters shall be determined by the street section in accordance with these standards.
- e. Permitted Tree Types/Species, that are suitable for urban streetscapes.
 - i. *Acer platanoides* 'Columnar' (Columnar Norway Maple)
 - ii. *Quercus robur* 'Fastigita' (Columnar English Oak)
 - iii. *Gleditsia triacanthos inermis* 'Skyline' (Thornless Common Honeylocust)
 - iv. *Pyrus calleryana* 'Aristocrat' (Aristocrat Pear)
 - v. *Pyrus calleryana* 'Redspire' (Redspire Pear)
 - vi. Others as approved by the Public Works department.

2. STREETScape PLANTER TYPE:

Photos are intended to represent examples of streetscape types and are not to scale.

a. Continuous Planter

The streetscape element of the Public Frontage that includes a continuous landscape area for the length of a block, unbroken by any area of non-living material.



SECTION VII. STREETScape AND LANDSCAPE

Interchange Overlay Development Standards

b. **Intermittent Planter**

The streetscape element of the Public Frontage, generally located along commercial streets or in areas with parallel parking, that provides for alternating areas of landscape and non-living landscape elements. The landscape areas shall be a minimum of 7 feet in length, alternated with areas of non-living landscape elements (such as pavers or stone) that shall be a maximum of 7 feet in length, located so as to allow ease of ingress and egress from parked cars.



SECTION VII. STREETScape AND LANDSCAPE

Interchange Overlay Development Standards

c. Tree Wells

Tree wells are not the preferred method of providing street trees; however, if they are proposed as an integral part of a streetscape that meets the Town's vision for the street, they may be permitted provided the soil requirements are met.



SECTION VII. STREETScape AND LANDSCAPE

Interchange Overlay Development Standards

3. STREETScape TABLE

a.

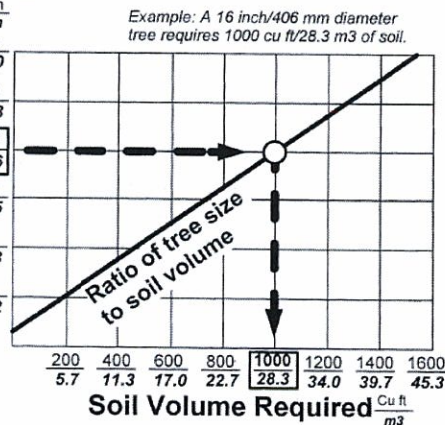
Planter Areas	Tree Size and Spacing	Irrigation Type
10 feet and greater	Shade Trees located 35 feet on center	Spray, drip or SDI
4-10 feet	Ornamental Trees	Drip or SDI
Less than 4 feet	Non-living material***	Drip or SDI

i. ***Generally only non-living material is permitted in these areas, however 5-foot square tree wells with ornamental trees and/or ground cover and low clearance shrubs with a mature height less than 4 feet without pruning, may be considered appropriate in these locations. No turf or overhead irrigation is permitted. Trees in these areas shall require one of the following:

- a. Structural soil is utilized, which provides a large volume of non-compacted soil with adequate drainage and aeration and reasonable fertility while fulfilling load-bearing requirements for base courses under pavement, or
- b. Soil is imported and made consistent with the graphic below.

Ultimate tree size

Crown Spread	DBH-Trunk Diameter
Sq Ft	Inch
m ²	mm
1200	24
111	610
1000	20
92	508
800	16
74	406
550	12
51	305
350	8
32	203
150	4
14	102



Calculating soil volumes for each tree

4. LANDSCAPING

- a. Xeriscape landscaping concepts for water conservation shall be incorporated by utilizing low water use plant material and organic mulches in landscaped areas.
- b. All landscaping shall be installed in conformity with the Town's line-of-sight regulations and with the Town's Landscape Regulations.
- c. Landscaping should be grouped in zones rather than randomly dispersed throughout the site. These zones could include entryways, corners, buffer zones, and locations to frame the building or accent the architecture.
- d. Street trees shall be provided in front of buildings except where entrances, special architectural features or storefronts require more exposure to the street.
- e. Entryways should be designed and planted with either ornamental or native plant material, depending on the theme of the center or project.

SECTION VII. STREETScape AND LANDSCAPE

Interchange Overlay Development Standards

- f. When low points on the site are close to the street edge, the appearance of the detention areas shall be carefully considered. Landscaping shall extend to the edge of the detention areas. Wherever possible, there shall be shared detention areas or larger regional detention areas.
- g. New developments should provide for opportunities for the installation of art in landscaped areas and in front of buildings.
- h. Adding color through the use of annual and perennial plant material is encouraged. This can occur by using potted plants in movable containers, which can be used to define outdoor seating areas, entries, and to define walkways.
- i. Where fencing of a property is required along a front property line or exterior side yard property line, it shall be located behind or integrated with the landscape buffer or screen. Landscape screens can be a combination of berms and plant materials and shall be thickly planted so as to obscure undesirable views.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

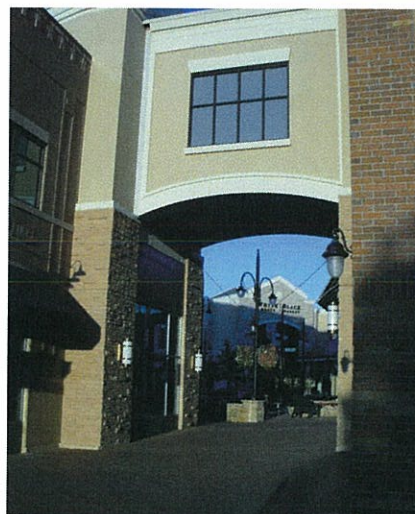
VIII. DESIGN STANDARDS

A. DESIGN STATEMENT

1. The depictions of Private Frontages are graphic only and are not intended to identify plant species, median or sidewalk spacing, or soil preparation. Reference the Town of Castle Rock Landscape Regulations or the streetscape standards.

B. SITE DESIGN AND LAYOUT

1. Buildings shall be sited and designed so as to frame and preserve views, as well as frame gateways along the corridors.
2. Buildings shall be designed to take up natural grade transitions and contours. Where this cannot occur, stepped retaining walls with landscaping shall be used.
3. Buildings shall be compatible in terms of scale, lot size, massing, building placement and orientation.
4. Orientation of new buildings shall take into account adjacent buildings and the angle of the sun in order to minimize ice build up on pedestrian or vehicular ways.
5. In a town center, where storefronts are typical, the building fronts shall be located at the property line to define the sidewalk edge and create a sense of vitality for the public sidewalk.
6. Buildings shall be located along the perimeter of a development with internal roadways and parking screened by buildings from the public roads. The importance of spaces between buildings should be recognized, and these spaces should have a planned and useful shape and not simply be left over areas.

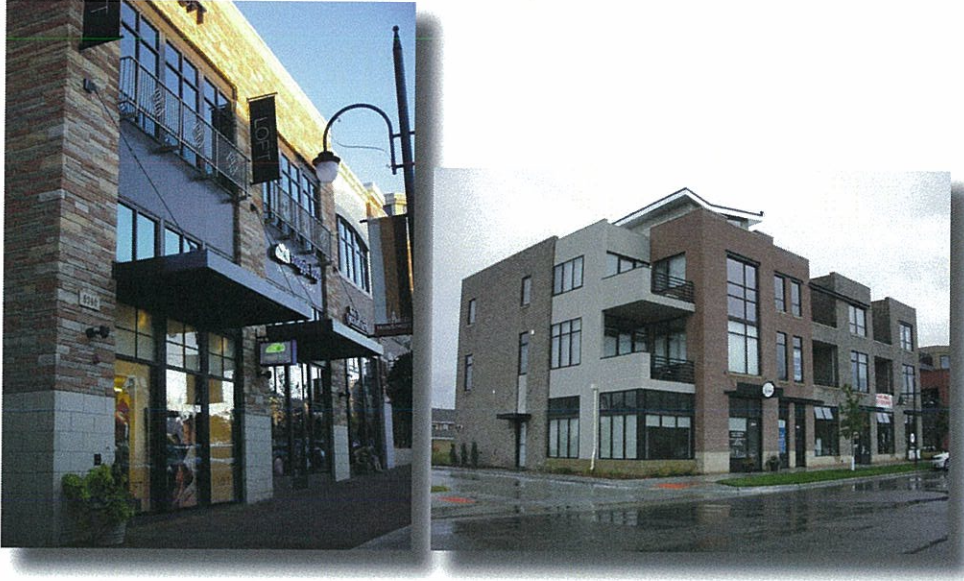


7. Facades shall be built parallel to a rectilinear Principal Frontage line or to the tangent of a curved Principal Frontage line, and shall include a portion of the structure at the setback line.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

8. Large clear windows, prominent entryways, awnings and canopies should be used.



9. Buildings shall have their primary orientation toward the street rather than the parking area.



10. Pedestrian walkways from the public sidewalk to the main building entrance shall be provided.
11. In the absence of a building facade along any part of a Frontage Line, a streetscreen shall be built parallel with the facade. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

12. Developments can be set back from the property line to allow for outdoor dining areas, patios, plazas, and entranceways as long as the facade continuity is not interrupted. Buildings that are placed immediately adjacent to the sidewalk should promote visibility and pedestrian orientation with plazas or other pedestrian areas.

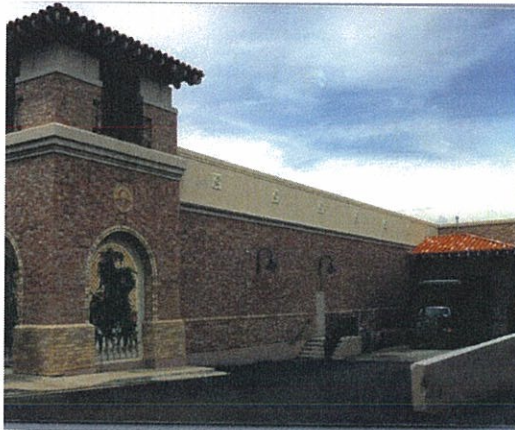


13. Trash and outdoor storage areas, mechanical equipment and similar areas shall be screened and shall be substantially obscured from the street. Attractive fences or walls shall be used to screen dumpsters and trash enclosures.





14. Roof-mounted mechanical equipment shall be concealed from street level public view by screening in a manner consistent with the character of the building.
15. Service areas and loading docks shall be screened or landscaped and should not be accessed directly from the street. Where feasible, service access and loading areas should be accessible from alleys or from parking lots located at the rear of buildings.



C. PARKING AND LOT DESIGN

1. Shared parking between adjacent businesses and/or developments is encouraged.
2. Vehicular access to a parking area should be provided from an alley or side street, where feasible, not from a primary shopping street.
3. Ground floor businesses are encouraged to permit access for patrons from the parking areas located at the rear of the building. Business signs and decorative lighting at this entry should be oriented to the pedestrian.
4. Parking lots shall provide a well-defined pedestrian circulation system within the site. Protected pedestrian walkways should directly link to entrances and the internal circulation of the buildings and to parking areas, and also to other buildings.
5. Trees, shrubs and ground covers shall be used in islands to break up large expanses of paving and provide shade, in conformance with the Town's Landscape Regulations. Water-efficient landscaping shall be used.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards



6. The landscape character of the streetscape should be extended into surface parking lots by introducing large canopy trees. The planting of shade trees in parking lot islands is required.



7. The preferred parking configuration is to have large-scale parking areas for a building or buildings located behind the building and be screened from view of adjacent properties with landscaping and decorative fencing. Where possible, car park screening shall follow the existing building line.



8. Parking areas should be separated from buildings by either walkways and/or landscaped strips. Parking with head-in orientation is acceptable where it can be shown

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

that the sidewalks are sufficiently wide to create a pleasant pedestrian area, the parking layout assists in reducing traffic speeds yet does not produce hazardous situations, and the overall character is beneficial to the pedestrian.

9. Access to parking structures should minimize disruption to storefront continuity and not conflict with pedestrian safety.
10. Where applicable, the design of decked parking and parking structures should be integrated with surrounding development in order to create a continuous, attractive streetscape.



D. ARCHITECTURAL DETAIL

1. Development that is visible from, or adjacent to roads, open space and residential areas shall have well designed façades on all sides. This is considered a "360 degree" design treatment.



SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

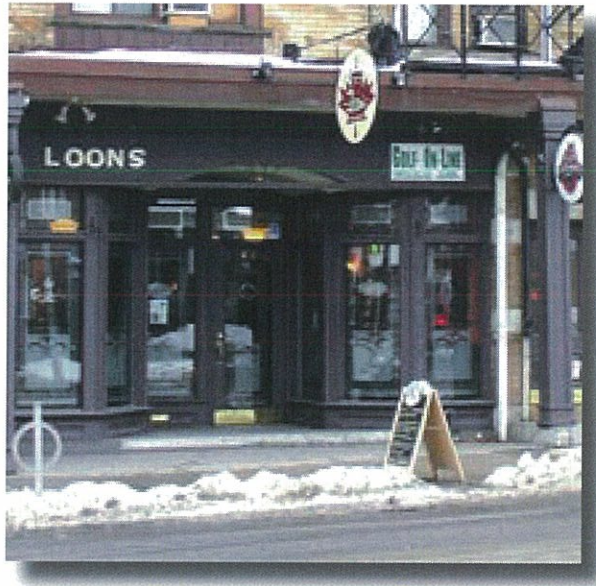
2. All development which is located within 10ft of a public street shall provide at minimum 50% of its ground floor length and twenty-five percent (25%) of the area of the ground level wall area, which is defined as the area up to the finished ceiling height of the fronting space or fifteen feet (15') above finished grade, whichever is less, as windows, display areas or doorways. Required windows shall allow views into lobbies or similar areas of activity, pedestrian entrances, or display windows. Required windows shall provide a lower sill no more than three feet (3') above grade; except where interior floor levels prohibit such placement, the sill may be located not less than two feet (2') above the finished floor level to a maximum sill height of five feet (5') above exterior grade. The size and scale of windows should be varied. Sills and mullions should be used to continue and emphasize a specific architectural style. Features such as recessed windows, pilasters, definition of floor lines, corner quoins, cornices, and parapets should be used to create shadows and visual interest.



3. The different elements of a building's facade should be emphasized by use of color, arrangement of facade elements, or a change of materials.
4. Clear, transparent windows shall be used for all ground floor retail uses.
5. Darkly tinted windows and mirrored windows which block two way visibility are prohibited as ground floor windows
6. The use of awnings is encouraged and awnings should fit in proportion to the module of the individual storefront rather than extending beyond a single bay.
7. Recessed entries to individual storefronts are required, as it creates a transition from the sidewalk to the interior of a Commercial building.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards



8. Awnings, arcades, and galleries may encroach the sidewalk to within two feet of the curb providing that the structures clear the sidewalk vertically by at least 8 feet.
9. Parapets shall be used to screen flat roofs and shall be designed as an architectural feature. A distinctive cornice can be used to emphasize a roofline in the design of a building.
10. Buildings at corners shall have special roof shapes in order to emphasize their corner location
11. Roof overhangs at pedestrian entries provide protection for shoppers and are encouraged.
12. Roofing materials shall be of a color and material consistent with the architectural character of the building and should convey a sense of permanence and quality.

E. BUILDING MASSING AND SCALE

1. Corner locations on a block are highly visible and shall be designed with visually prominent elements and variation in height. Corner entryways are encouraged, and alcoves, canopies, or other means of shelter should be provided at corner intersections.



SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

2. Expression lines shall be utilized for buildings three stories or greater in height.
3. Variation in the building façade by vertical or horizontal articulation, window and entry variations, patios, plazas or other landscaped pedestrian areas is encouraged. Strong vertical elements such as windows, pilasters, columns, stairs, and towers shall be used where feasible to identify individual Commercial spaces.
4. Variations in roofline and building height can effectively break up massing and provide visual interest. Variations in rooflines can include gables, dormers and well-defined parapets.



5. The primary entrances to the building should be clearly identified with a canopy or awning, or treatments on the public sidewalk could also be used to define the major entries.
6. Large volumes or planes shall be broken up into smaller ones in order to reduce the visual scale of a building. The mass of a building shall be varied in form or divided to emphasize the various interior building functions.



7. The upper stories of a building shall be distinguished by using offsets or changes of material.
8. The sidewalk in front of a building shall be designed with elements that create a pedestrian friendly environment. Design elements shall be used to visually reduce the mass of the building.

F. BUILDING MATERIALS AND COLOR

1. Building or accent colors shall not be bright or intense nor should highly reflective surfaces be utilized. Colors of the building materials should reflect those found in the natural landscape, such as soft greens, muted reds and pinks, warm grays and brown tones.
2. Materials such as stone, brick, pre-cast concrete, cast stone and architectural metals should be combined to enrich the appearance of a building and highlight architectural features.



3. Building materials and texture on the ground floor add to the pedestrian experience and quality finish materials shall be used. Compatible materials shall be used on all sides of a building.
4. Within a development, the materials, texture and color of materials of the individual storefronts should be varied. Contrasting colors for architectural details, awnings, and at entrances shall be used to create interesting architectural features.



5. Large areas of white or cool grays, and reflective glass curtain wall systems may be permitted by variance.
6. Bright corporate colors may be permitted by variance.

SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

G. SIGNAGE

1. Hanging signs can use a wide variety of colors and icons to create a unique character and are encouraged.



2. Signs shall have a minimum clearance of eight feet above the sidewalk for public safety.
3. Signs shall be incorporated into the architecture of the building.
4. Internally illuminated signs are not recommended.
5. All permanent external signage should be designed and located in the landscape so as to be compatible with the building design and scale.

H. LIGHTING

1. Innovative, decorative, and attractive light fixtures are encouraged and shall be used throughout the development. These fixtures shall fit the style of the building and respect the Town's character.



SECTION VIII. DESIGN STANDARDS

Interchange Overlay Development Standards

2. Pedestrian-oriented light fixtures shall be incorporated into the design of open spaces and should be complementary to the existing light standards on the sidewalks.



3. Lighting of the development shall complement the building design. Floodlights can be incorporated into the design of the building façade and shall be cut-off fixtures with reduced glare.
4. Lighting that is non-IREA approved shall be maintained privately and not by the Town.
5. Use of LED lighting technologies are encouraged.

I. WALLS AND FENCES

1. Large expanses of blank retaining walls are not permitted. Variations in the color of the wall, materials and landscaping are all to be used, singly or in combination, to improve the appearance of a wall.
2. Retaining walls shall be distributed throughout the site rather than be designed as high perimeter walls. (Terracing may be an option on a case by case review.)
3. Landscape pockets along walls or fences are encouraged. These should be a design element that is repeated frequently along the length of a wall.

SECTION III. DESIGN STANDARDS

Interchange Overlay Development Standards



4. Walls comprised of two or more retaining walls are required to include landscaping at the base, the terrace between walls, and at the top of a retaining wall. Trees or shrubs shall be planted in indentations in the wall or landscaped areas in front of the wall.
5. Retaining walls shall be designed to blend in with the adjacent buildings or structures unless shielded from public view by adjacent buildings or structures.
6. A decorative cap, railing or some similar element shall be placed at the top of a retaining wall.

VIII. DEFINITIONS

This Article provides definitions for terms in this Code that are technical in nature or that otherwise may not reflect a common usage of the term. If a term is not defined in this Article, then it is located within the Town of Castle Rock's Municipal Code. If the definition is not located within the Town of Castle Rock's Municipal Code the Town of Castle Rock's Development Services Director will determine its meaning and intent.

Apartment: a residential unit sharing a building and a lot with other units and/or uses; may be for rent, or for sale as a condominium.

Block Perimeter: A total measurement of the boundaries of all sides of a unit of contiguous lots or parcels bounded by roadways or other barriers.

Building Situation: the placement of a building on its lot.

By Right: characterizing a proposal or component of a proposal for a IOZ PD that complies with the interchange overlay zoning development standards and is permitted and processed administratively, without public hearing.

Code: Town of Castle Rock's Interchange Overlay Zoning Development Standards.

Commercial: the term collectively defining workplace, Office, retail, and lodging functions.

Configuration: the form of a building, based on its massing, Private Frontage, and height.

Corridor: a lineal geographic system incorporating transportation and/or greenway trajectories.

Edgeward Building: a building that occupies the center of its lot with setbacks on all sides.

Effective Parking: the amount of parking required for Mixed-Use after adjustment by the shared parking factor.

Encroachment: any structural element that breaks the plane of a vertical or horizontal regulatory limit, extending into a setback, into the Public Frontage, or above a height limit.

Expression Line: a line which breaks up the façade of a building over 3 stories which extends across the major part of the width of a façade and is expressed by a variation in material or by a limited projection such as a molding or balcony. (Syn: transition line.)

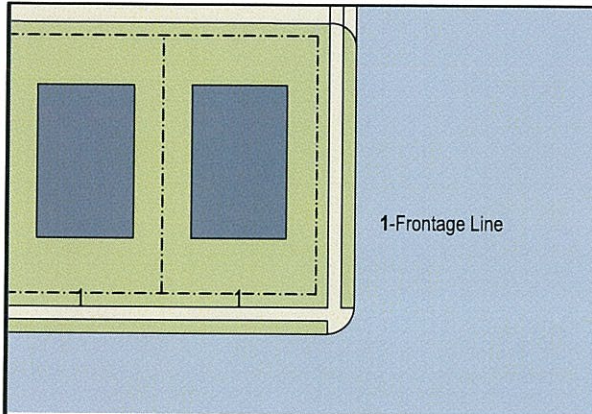
Flex Building: A building that includes a mixture of light industrial, office and retail spaces, including the opportunities for warehouse or light manufacturing operations, related office and a showroom/retail space in a shopfront type building.

SECTION IX. DEFINITION OF TERMS

Interchange Overlay Development Standards

Frontage: the area between a building facade and the vehicular lanes, inclusive of its built and planted components. Frontage is divided into Private Frontage and Public Frontage.

Frontage Line: a lot line bordering a Public Frontage. Facades facing Frontage Lines define the public realm and are therefore more regulated than the elevations facing other lot lines.



Function: the use or uses accommodated by a building and its lot.

Gateway: An entrance that defines the Town's boundaries and points of entry from major streets, alleys, sidewalks and bikeways and can consist of an ensemble of streets, buildings, natural features and landscapes, bridges, and special elements such as signage and public art. Gateways can provide a strong sense of identity and arrival to the Town. The major gateways into Castle Rock along major streets, alleys, sidewalks and bikeways should convey the Town's identity by utilizing characteristic design elements that reflect Castle Rock's character and heritage.

Green: a civic space type for unstructured recreation, spatially defined by landscaping rather than building Frontages.

Greenway: an open space corridor in largely natural conditions which may include trails for bicycles and pedestrians.

IO: Interchange Overlay District refers to Chapter 17.16 of the Town's municipal code.

IO PD: Interchange Overlay Planned Development that has been approved by the Town of Castle Rock's Town Council.

Interchange Overlay District (IOD): is identified in the Town's adopted 2020 Comprehensive Master Plan, as amended, that is one of four existing or proposed interchanges along I-25 as locations for gateways into the town where higher intensity and high quality economic development should occur through zoning initiatives. An outline zoning overlay of existing zoning entitlements that is voluntary and implemented through the IOZ PD process.

Landscape Regulations: those regulations, criteria, manuals and other documents that govern landscaping.

SECTION IX. DEFINITION OF TERMS

Interchange Overlay Development Standards

Light Industrial: A land use that consists of activities such as manufacturing, research and development, processing, fabrication, wholesaling, and/or storage. A retail component may be permitted in conjunction with these activities.

Liner Building: a building specifically designed to mask a parking lot or a parking structure from a Frontage.

Live-Work Unit: a Mixed-use unit consisting of a Commercial and residential function. The Commercial function may be anywhere in the unit. It is intended to be occupied by a business operator who lives in the same structure that contains the Commercial activity or industry. (See *Work-Live*)

Manufacturing: premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery or artisan's equipment, and including their retail sale.

Mixed-use: multiple functions separated horizontally or vertically within the same or multiple buildings.

Office: premises available for the transaction of general business but excluding retail and Manufacturing uses.

Parking Structure: a building containing of one or more stories of parking above grade.

Plaza: a civic space type designed for civic purposes and Commercial activities. They are generally paved and spatially defined by building Frontages.

Pocket Park: an open space area centrally located within identified pedestrian shed, owned and maintained by an HOA or other private entity, designed and equipped for the recreation of children that includes playground equipment and open plan areas.

Principal Building: the main building on a lot, usually located toward the Frontage.

Principal Entrance: the main point of access for pedestrians into a building.

Principal Frontage: On corner Lots, the Private Frontage designated to bear the address and Principal Entrance to the building, and the measure of minimum lot width.

Private Frontage: the privately held Layer between the Frontage Line and the Principal Building facade.

Public Works Regulations: those regulations, criteria, manuals and other documents that govern public works infrastructure.

Recess Line: a line prescribed for the full width of a facade, above which there is a stepback of a minimum distance, such that the height to this line (not the overall building height) effectively defines the enclosure of the enfronting public space.

Transportation Master Plan: the master plan that governs all transportation elements and design criteria.

SECTION X. EXAMPLE SHARED PARKING CALCULATION

Interchange Overlay Development Standards

SHARED PARKING CALCULATION EXAMPLE

The example described here calculates the shared parking requirement for a mixed use development that comprises of the following:

- 60 Residential Units (peak parking demand primarily at night time and weekend)
- 5000 sq.ft of Office space (peak parking demand primarily at weekday)
- 10000 sq.ft of Retail space (peak parking demand primarily at weekend, weekday and evening)
- 75 bed Hotel (peak parking at night and weekend)
- 100 seat Restaurant.
- 500 sq.ft Medical Office (peak parking spread throughout the week)

The shared parking calculation takes into account the peak parking required for a combination of uses at any one time. In this example we can see that the peak parking requirement is at the Weekend (269 spaces). In normal parking code circumstances each use would be calculated separately, giving a parking requirement of 287 spaces. The shared parking code allows a maximum reduction of 20% (or as approved by the Development Services Director) if the shared peak parking warrants that. In this case the peak parking requirement shows a 6.3% reduction from normal requirements.

Interchange Overlay Zoning Development Parking Standards						
	Unit	Standard	Weekday	Evening	Night	Weekend
Residential (units)	60	120	54	90	120	120
Office (sq. ft)	5000	20	20	8	1	2
Retail	10000	35	34	35	2	35
Hotel (rooms)	75	75	72	57	75	75
Restaurant (seats)	100	34	34	34	6	34
Medical Office (sq. ft)	500	3	3	3	1	3
	Required	287	217	227	205	269
Standard Requirement	287					
Maximum Reduction (20%)	230					
Peak Demand	269	reduction				
IOZ Requirement	269	6.3%				



AGENDA MEMORANDUM

To: Honorable Mayor and Members of Town Council

From: Martin Hudson, Long Range Project Manager, Development Services

Title: Ordinance No. 2010-: An Ordinance Amending The Town Of Castle Rock Zoning District Map By Creating A New Interchange Overlay District Classification; Designating The Interchange Overlay District Boundaries; Adopting the Interchange Overlay Development Standards By Reference As A Primary Code; Adopting A New Chapter 17.16 And Amending 17.12.010 Of The Municipal Code Concerning The Interchange Overlay District; And Amending Chapter 17.14070 By Exempting Properties With An Approved Interchange Overlay Preliminary Planned Development Site Plan From The Skyline Ridgeline Protection Regulations.

Executive Summary

This ordinance was approved on first reading with a vote of 7 to 0 with two minor changes to the text and a request to view the Town's Skyline/Ridgeline map at the second reading.

In 2008, Town Council amended the Town of Castle Rock's 2020 Comprehensive Master Plan to include the concept of Interchange Overlay Districts (IOD). These districts were envisioned to allow greater density and urban development in regions surrounding the I-25 interchanges. In order to implement the 2020 Comprehensive Master Plan's concept of Interchange Overlay Districts, the development community and Town staff worked together to develop the Interchange Overlay District (IO) ordinance (**See Attachment A – Ordinance 17.16**) and its accompanying Interchange Overlay Development Standards (IODS). (**See Attachment B – Development Standards**) Some of the key elements in the IO and the IODS that will assist in the implementation of the 2020 Comprehensive Master Plan objectives are: reduced parking requirements, improved pedestrian connectivity, increased building heights, the allowance of mixed use and the potential to integrate housing in close proximity to employment and services. The IO is a voluntary zoning option for property owners and the Town.

Notification and Outreach Efforts

Town staff made public presentations to the Castle Rock Economic Development Council, the Castle Rock Economic Development Policy Committee and held a public input meeting for property owners. The IO and IODS have received positive support through these outreach efforts.

One area that the development community identified as needing future public dialog, is how public/private partnerships might assist in the construction of parking structures. This will be needed to fully implement the Interchange Overlay development concept of vertical density.

History of Past Town Council, Boards & Commissions, or Other Discussions

The idea behind creating an interchange overlay program was to capture commerce from the 70,000+ vehicles that travel I-25 on a daily basis and to strengthen the tax base for the residents of Castle Rock to further the Vision goals established by the community. It was noted that the character of Castle Rock is found through the main entry points to our community, and therefore, strengthening those areas would attract new business and establish our goal of a free-standing community.

On February 26, 2008 Town Council amended the Town of Castle Rock's 2020 Comprehensive Master Plan by adding the concept of Interchange Overlay Districts in areas around current and future I-25 interchanges. Since that time, several iterations of the text have been reviewed and updated to ensure we are meeting the Council goals outlined in the plan amendment. A number of community outreach programs and developer teammate meetings have occurred to ensure the code is well balanced and provides the opportunity to create commerce while maintaining the history and heritage of the Town Vision.

The Castle Rock EDC and development teams that own land adjacent to existing and proposed interchanges have accepted and support this effort.

Discussion

The overall goal of the proposed IO and IODS is to assist in the implementation of the Town's 2020 Comprehensive Plan and the community's goals. Some of the goals include:

- A strong economic base;
- Multimodal environment;
- Mixed-use development.

With these goals and commonalities in mind, the business community, Castle Rock EDC and staff have addressed zoning issues to allow the community's vision to become a reality. Some of the proposed zoning changes are:

- Because it creates intentional mixed use, the Residential/Non-residential Interface Regulations will not apply within the IO, but the Interface Regulations will still apply on the perimeter. This will allow the IO area to have the opportunity to develop into a more sustainable mixed use environment by allowing citizens to live in close proximity to an employment center;
- Increase allowed building heights to a maximum of six (6) stories or ninety (90) feet, in order to incentivize the development of these vital land uses;
- Requiring architectural and site layout feature that will enhance the visual look of the project, public safety, and the aesthetics of a building;
- Provide basic design criteria for landscaping, buildings, wall elevations, fencing, screening of outdoor storage;

- The Skyline Ridgeline Protection Regulations will not apply within the IO to allow for a more compact sustainable development pattern;
- A reduction in open space requirement from twenty (20%) percent to fifteen (15%) percent will assist in maximizing the economic value of land located around the I-25 interchanges;
- Reduce parking requirements incentivize the development/ redevelopment of properties and expand business opportunities in the area;
- Require pedestrian connectivity to businesses and developments, which will enhance the multi-modal transportation options in the IO.

The IO is a voluntary ordinance, allowing each property owner within the designated IO boundary, to request replacing the property's current zoning with an IO PD. All IO PD requests will follow the Town's standard planned development process, allowing Town Council to have the final say whether a property receives an IO PD.

Budget Impact

There is no negative budget implications to the Town related to the proposed IO and IODS.

Staff Recommendation

To adopt the proposed Chapter 17.16, the Interchange Overlay Design Standards, and the accompanying amendments to Chapter 17.14.070 and Chapter 17.12.010 which is designed to:

- Assist in the implementation of the Town of Castle Rock's 2020 Comprehensive Master Plan;
- Encourage the location of commercial, office, residential and industrial uses where adequate transportation facilities exist;
- Attract a variety of businesses, industries and employment opportunities to provide a balance in the community between jobs and workers and to diversify the Town's economic base;
- Promote multi-modal access to commercial and retail uses.

Proposed Motion

I move to approve Ordinance No. 2010-: ____, adopting Chapter 17.16, the Interchange Overlay Design Standards, and the accompanying amendments to Chapters 17.14.070 and 17.12.010.

Attachments

Attachment A – Interchange Overlay District Ordinance
Attachment B – Interchange Overlay Development Standards